

(DATE)

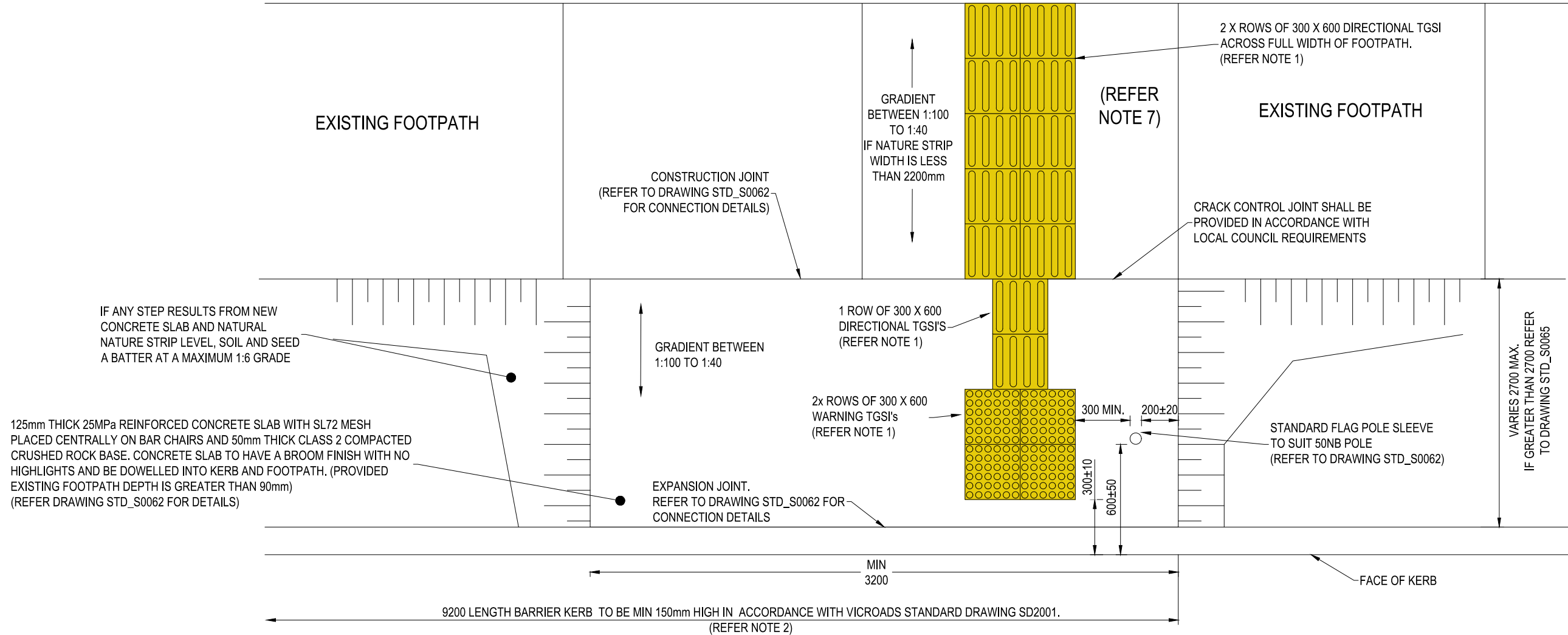
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Certified By:

REPLACE WHOLE PANEL OF FOOTPATH BETWEEN FOOTPATH CONSTRUCTION/CRACK CONTROL JOINTS IF LANDING GRADES EXCEED 1:40 OR IF EXISTING FOOTPATH IS IN POOR CONDITION. (REFER NOTE 4)

TYPE # 1



NOTES:

- DOT ONLY ACCEPTS YELLOW TGSIS, MECHANICALLY AND CHEMICALLY SECURED ON ANY STABLE SURFACE, AS PER MANUFACTURES SPECIFICATIONS. TGSIS MUST BE AN APPROVED PRODUCT BY VICROADS AS PER VICROADS DESIGN NOTE RDN06-05.
- A) RETAIN EXISTING KERB IF KERB IS A BARRIER KERB OF MINIMUM HEIGHT 150MM.
 B) IF THE EXISTING KERB IS NOT A BARRIER KERB, REPLACE THE EXISTING KERB WITH A BARRIER KERB MINIMUM 9.2M LENGTH AND A MINIMUM 150MM HIGH (MAX 190MM) FROM KERB INVERT. IF THE EXISTING KERB IS A BARRIER KERB WITH A HEIGHT LESS THAN 150MM FROM THE KERB INVERT, REPLACE THE BARRIER KERB UP TO THE LENGTH OF THE BUS STOP HARDSTAND. KERB INVERT TO BE ALIGNED.
 C) WHERE PEDESTRIAN FENCE IS REQUIRED, EXTEND THE HARDSTAND BY 150MM.
 D) KERB TRANSITION SHALL BE PROVIDED IN ACCORDANCE WITH VICROADS STANDARD DRAWING SD2102.
 E) KERB RETURN SHALL BE PROVIDED IN ACCORDANCE WITH VICROADS STANDARD DRAWING SD2101.
 F) IF A DRIVEWAY IS SITUATED WITHIN 9M OF THE BUS STOP FLAG POLE ON APPROACH, THE DRIVEWAY WILL NEED TO REMAIN, SO ANY NEW BARRIER KERB SHALL BE TIED INTO THE EDGE OF DRIVEWAY.
- ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE NOTED.
- ALL CONSTRUCTED CONCRETE FOOTPATHS SHOULD BE BUILT TO LOCAL COUNCIL FOOTPATH STANDARDS. WHERE EXISTING FOOTPATH IS ASPHALT, THE MINIMUM PANEL WIDTH TO BE REPLACED WITH CONCRETE IS 1200mm
- DRAWINGS STD_S0062 - STD_S0081 SHALL BE READ IN CONJUNCTION WITH EACH OTHER.
- DIMENSIONS MUST BE A MINIMUM 300 BETWEEN TACTILE EDGE AND POLE EDGE. PREFERABLY BELOW 400, OR AS APPROVED BY DOT-BUS INFRASTRUCTURE TEAM.
- IF GRADE 1:40 CANNOT BE ACHIEVED ON LANDING, THE FOOTPATH WILL NEED TO BE LOWERED/RAISED AND TRANSITION GRADE TO BE BETWEEN 1:14 AND 1:20, AS PER COUNCIL REQUIREMENTS.
- ENSURE INVERT OF NEW KERB IS ALIGNED AND CONTINUES WITH EXISTING KERB.
- EXPANSION JOINT IS REQUIRED IF HARDSTAND IS MORE THAN 6M LONG.

ORGANISATION	AUTHORISED BY	DATE
DOT		

Revised By	In Serv	Rev.	Date	Description	Designed	Checked	Ind. Review	Approved
TOMKINSON		E	14/10/21	AMENDMENTS AS PER DOT COMMENTS	OB	JH	MM	MM
TOMKINSON		D	11/10/21	AMENDMENTS AS PER DOT COMMENTS	OB	JH	MM	MM
TOMKINSON		C	07/02/20	AS PER PTV COMMENTS	JG	JG	RQ	JG
TOMKINSON		B	20/01/20	AS PER PTV COMMENTS	JG	JG	RQ	JG
TOMKINSON		A	23/07/19	AS SUBMITTED TO PTV	JG	JG	RQ	JG

Consultant	
Franchisee / Lessee	

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All written dimensions take precedence over scaled dimensions.

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STRUCTURAL STANDARD DRAWING		
BUS STOP FOR A SITE WITH NATURE STRIP BETWEEN FOOTPATH & KERB (MAX NATURE STRIP WIDTH OF 2700mm)		
Up Location	Down Location	Datum
East . . .	East . . .	
North . . .	North . . .	
ID# . . .	ID# . . .	

PUBLIC TRANSPORT VICTORIA PT	
File Name	STD_S0064_RevA.DWG
Sheet No.	1 of 1
In Serv.	
Scale	1:25
Sheet Size	A3

Drawn By	OB	Designed By	OB
Checked By	JH	Ind. Review	MM
Approved	MM	Approval Date	14/10/21
Drawing Number	STD_S0064	Revision	E

23/07/19