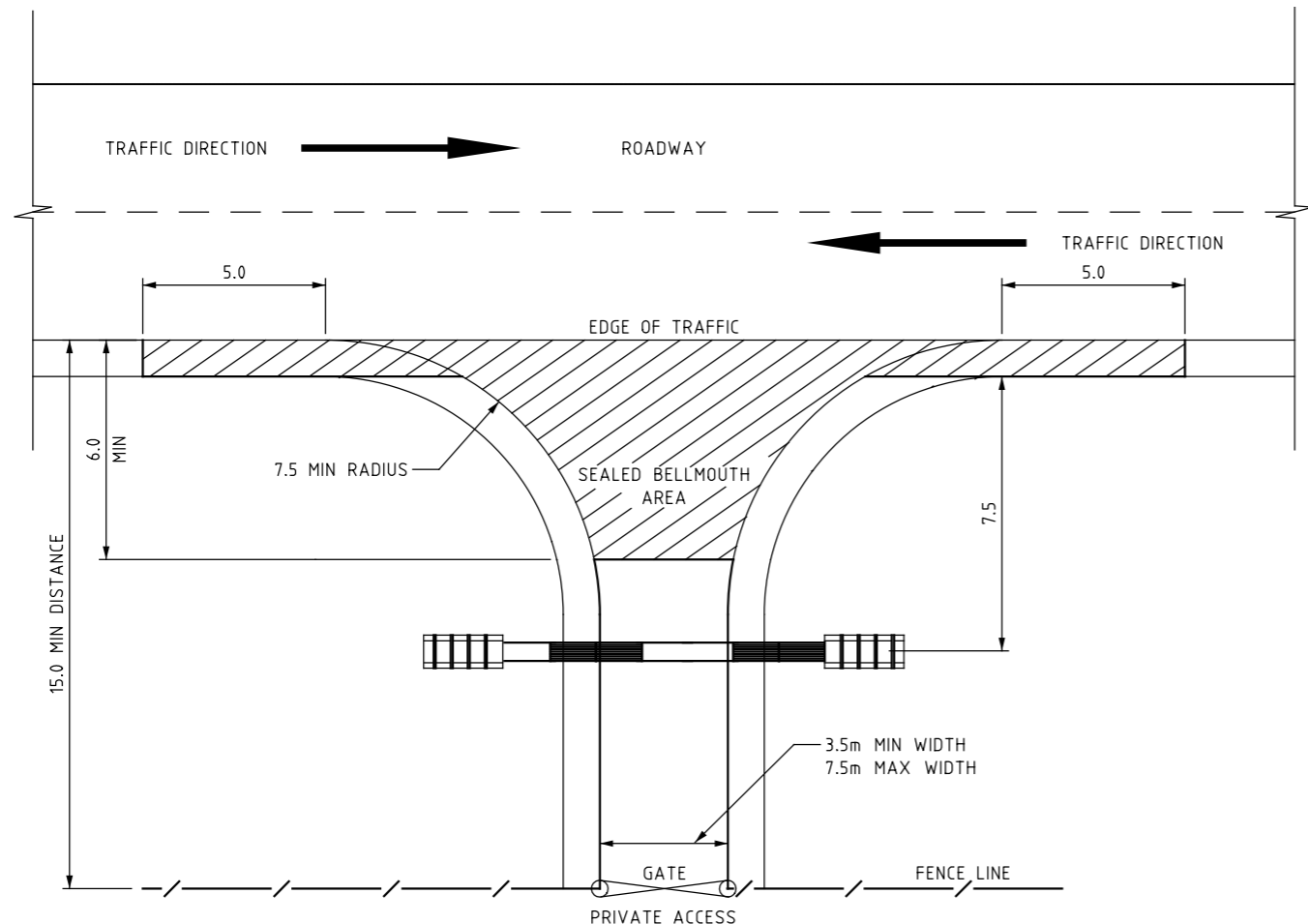
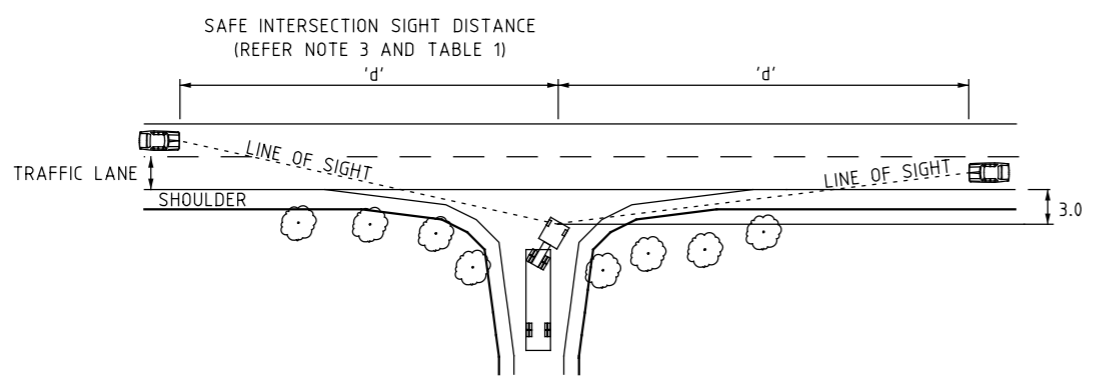


ISSUE	APP'D	DATE	AMENDMENT
A	DC	08/12	GENERAL DETAIL & NOTES



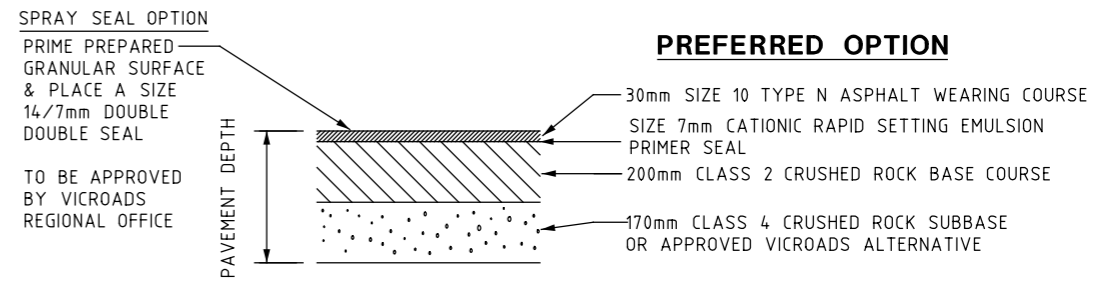
PLAN - TYPICAL DRIVEWAY ACCESS



**TABLE 1**

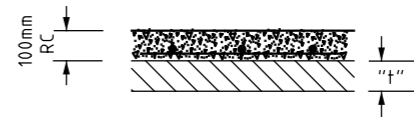
DESIGN SPEED (Km/h)	'd' SAFE INTERSECTION SIGHT DISTANCE (m)
60	123
70	151
80	181
90	226
100	262
110	300

TYPICAL FLEXIBLE PAVEMENT COMPOSITION



NOTE: ACCESS DRIVEWAY PAVEMENT DEPTH SHOULD BE DETERMINED FROM EXISTING PAVEMENT AND A SOIL TEST ANALYSIS

RIGID PAVEMENT OPTIONS



BASE COURSE THICKNESS "t" SHOULD BE DETERMINED FROM SOIL TEST ANALYSIS

**OPTION A**  
125mm CONCRETE WITH F72 MESH ON CLASS 2A CRUSHED ROCK BASE COURSE

PAVEMENT DESIGN

PAVEMENT DESIGN SHOULD BE IN ACCORDANCE WITH VICROADS CODE OF PRACTICE RC 500.22 - SELECTION AND DESIGN OF PAVEMENTS AND SURFACINGS AND AUSTRROADS GUIDE TO PAVEMENT TECHNOLOGY PART 2 - PAVEMENT STRUCTURAL DESIGN.

RESIDENTIAL DRIVEWAY ACCESS LAYOUT GUIDELINES

GENERAL NOTES:

1. PAVEMENT COMPOSITION  
THE DIAGRAMS ARE A GUIDE FOR A TYPICAL LAYOUT OF A DRIVEWAY ACCESS FOR A RURAL PROPERTY. THIS GUIDE WAS DEVELOPED IN THE INTEREST OF ROAD SAFETY AND TO PROTECT THE ROAD PAVEMENT AT THE ACCESS LOCATION. FINAL PAVEMENT COMPOSITION SHOULD BE DETERMINED BY EXISTING PAVEMENT AND SOIL TESTS AND APPROVED BY VICROADS REGIONAL OFFICE.
2. A SEALED BELLMOUTH AREA SHALL EXTEND A MINIMUM OF 6.0m FROM THE EDGE OF THE TRAFFIC LANE AS SHOWN BY THE HATCHED AREA.
3. A PLANNING PERMIT IS REQUIRED FOR A NEW ACCESS OR ALTERATION TO AN EXISTING DRIVEWAY AND MAY BE REQUIRED FOR THE REMOVAL OF NATIVE VEGETATION.
4. A TRAFFIC MANAGEMENT PLAN MUST COMPLY WITH THE RMA 2004 CODE OF PRACTICE FOR WORKSITE SAFETY TRAFFIC MANAGEMENT IN RELATION TO ANY WORKS UNDERTAKEN WITHIN THE ROAD RESERVE.
5. THE DRIVEWAY ACCESS CONSTRUCTION AND MAINTENANCE IS THE RESPONSIBILITY OF THE PROPERTY OWNER. MAINTENANCE ALSO INCLUDES ASSOCIATED DRAINAGE WORKS.

SIGHT DISTANCE

THE TRUCK DRIVER WHEN LOCATED 3m FROM THE EDGE OF THE TRAFFIC LANE NEEDS TO BE ABLE TO SEE A VEHICLE APPROACHING IN EITHER DIRECTION WHEN EXITING FROM A DRIVEWAY. REFER TO TABLE 1 FOR APPROPRIATE SIGHT DISTANCES. TREE CANOPIES, BUSHES OR OTHER OBJECTS SHALL BE REMOVED TO PROVIDE GOOD VISIBILITY AND TO PREVENT TRUCK DAMAGE. ANY TREE CANOPIES OVERHANGING THE PATH OF A TRUCK SHALL BE A MINIMUM OF 5m ABOVE THE GROUND SURFACE.

VICROADS DRAWING NO 720260

- REFERENCES AND NOTES:
1. ALL DIMENSIONS ARE IN METRES
  2. AGRD PART 4 AND VR SUPPLEMENT TO AGRD PART 4 (PROPERTY ACCESS AND MEDIAN OPENINGS)
  3. AGRD PART 4A AND VR SUPPLEMENT TO AGRD PART 4A (SECTION 3 SIGHT DISTANCE AND TABLE 3.2)
  4. DESIGNED FOR 5.2m CAR, 12.5m SINGLE VEHICLE

VICROADS GUIDELINES FOR ACCESS TO RURAL PROPERTIES  
AGAM PART 5  
AGPT PART 2  
VICROADS TEM VOL 2  
VICROADS RC 500.22  
SD 1991 DRIVEABLE CULVERT ENDWALLS - TYPE 1

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**ACCESS & STOPPING BAYS SD2066**  
TYPICAL RURAL DRIVEWAY ACCESS TO  
RESIDENTIAL PROPERTIES - TYPE C

0 2 4	APPROVED DANIEL CASSAR (PRDE) 13-8-12	SD NO. SD2066	ISSUE A
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