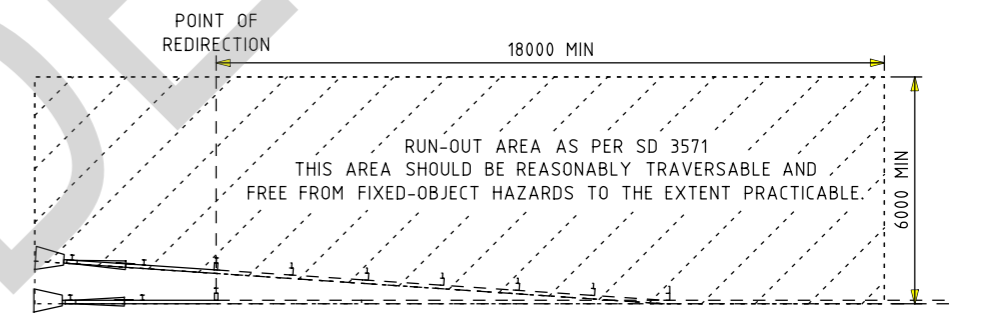
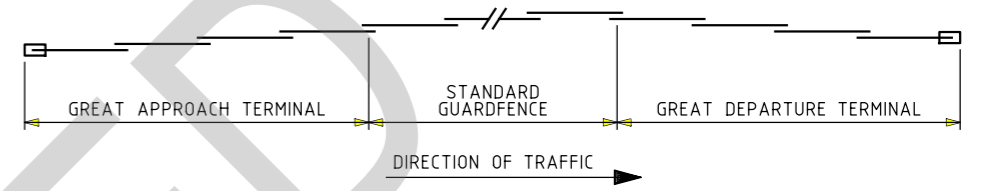
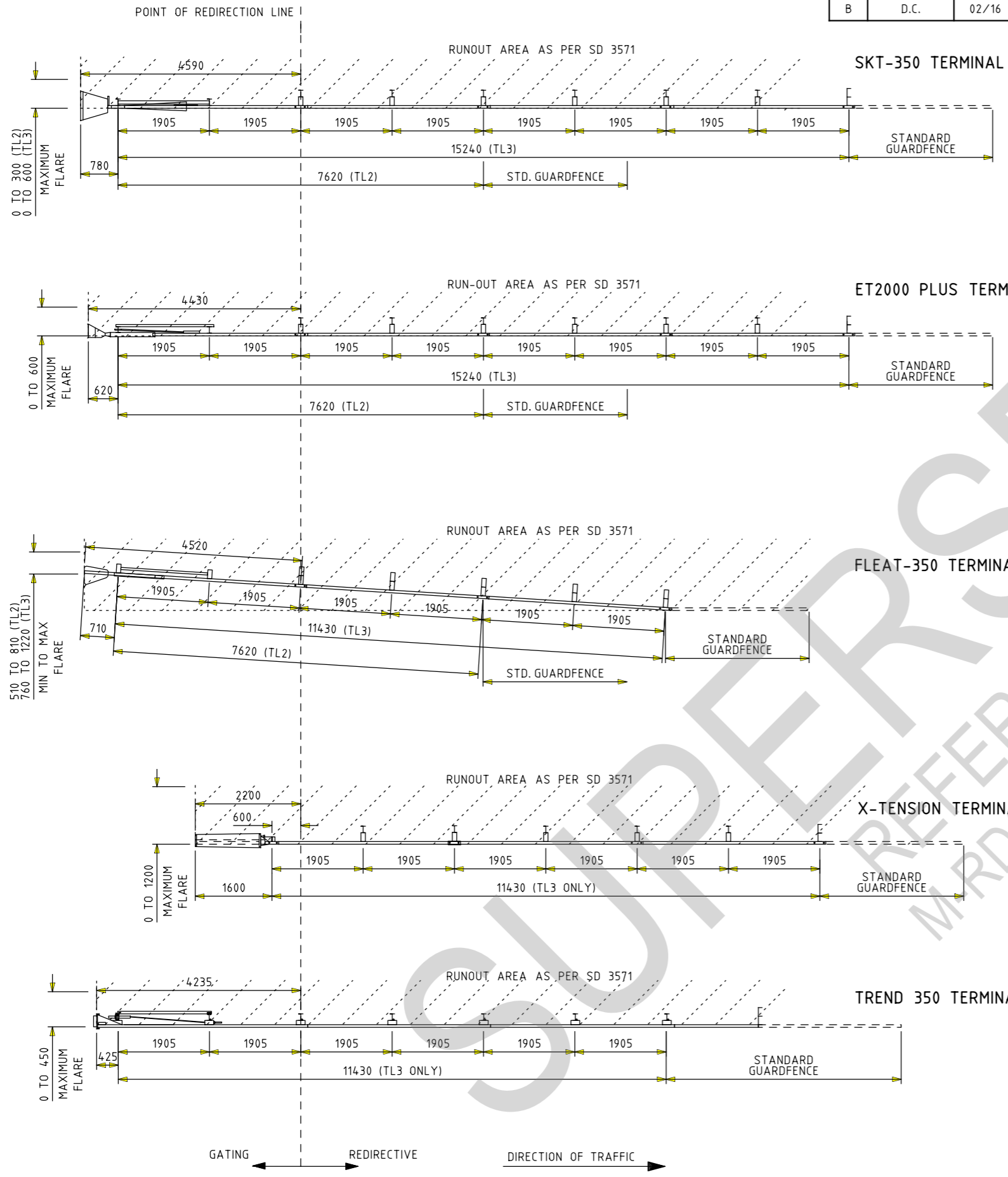
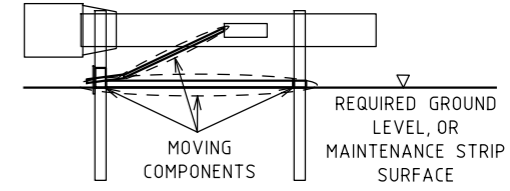


ISSUE	APPROVED BY	DATE	AMENDMENT
B	D.C.	02/16	TREND 350 TERMINAL ADDED



- NOTES:**
1. RUNOUT AREA REQUIREMENTS SHALL BE IN ACCORDANCE WITH SD3571.
 2. CROSS SLOPE OF RUNOUT AREA PREFERABLY SHOULD BE 10 TO 1 OR FLATTER. GRADING DETAILS ON SD 3571 ARE MINIMUM REQUIREMENTS AND ONLY APPLY TO THE SITUATION WHERE THIS IS IMPRACTICAL.
 3. WHERE THE MINIMUM RUNOUT AREA AS SPECIFIED IN SD3571 IS NOT ACHIEVABLE, CONSIDERATION IN ORDER OF PRECEDENCE SHALL BE GIVEN TO:
 - (i) EXTENDING THE BARRIER UPSTREAM OF THE PROPOSED LOCATION TO ACHIEVE THE MINIMUM RUNOUT AREA IN ACCORDANCE WITH SD3571;
 - (ii) PROVIDING THE MAXIMUM ACHIEVABLE RUNOUT AREA GIVEN EXISTING SITE CONSTRAINTS ALSO SUPPORTED WITH A DOCUMENTED RISK EVALUATION; OR
 - (iii) THE AREA SHOULD AT LEAST BE SIMILAR IN CHARACTER TO THE ADJACENT UNSHEILD ROADSIDE AREA, SUPPORTED WITH A DOCUMENTED RISK EVALUATION.
 4. 'POINT OF REDIRECTION' IS THE POINT AT WHICH THE TERMINAL IS CONSIDERED RE-DIRECTIVE.
 5. 'FLARED' TERMINALS SHALL BE INSTALLED AS A STRAIGHT ALIGNMENT AND OFFSET BUT NOT CURVED OVER THE LENGTH OF THE TERMINAL.
 6. TL2 - TEST LEVEL 2 SUITABLE FOR OPERATING SPEEDS UP TO 70KM/H, TL3 - TEST LEVEL 3 SUITABLE FOR OPERATING SPEEDS GREATER THAN 70KM/H.
 7. REFER TO PRODUCT MANUAL FOR INSTALLATION REQUIREMENTS.
 8. ONLY TERMINALS THAT ARE APPROVED BY VICROADS AND MEET THE REQUIREMENTS OF NCHRP 350 SHALL BE USED.
 9. WHERE SPECIFIED OR SHOWN ON THE DRAWINGS, MAINTENANCE STRIPS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 708 AND PARTICULAR ATTENTION SHALL BE PAID TO ENSURE THAT ANY ABOVE GROUND LEVEL MOVING COMPONENTS SUCH AS GROUND STRUTS OR CABLES ARE NOT RESTRICTED FROM FUNCTIONING BY CONCRETE PLACEMENT.
 10. BARRIERS LOCATED WITHIN 1.5m OF ROAD PAVEMENT AND BEHIND K&C HAVE A HEIGHT MEASURED TO PAVEMENT PLACING MOVING COMPONENTS BELOW ACTUAL GROUND LEVEL. CONCRETE MAINTENANCE STRIPS SHALL BE MOULDED AROUND LOWERED GROUND STRUTS AND POST CONNECTIONS WITHIN POST 1 AND 2 TO ENSURE TERMINALS PERFORM AS REQUIRED.
 11. WHEN CONSIDERING A FLARED TERMINAL BEHIND KERB AND CHANNEL THAT CANNOT MEET THE REQUIREMENTS OF SD3502, NOTE 3 ABOVE DOES NOT APPLY AND A MINIMUM RUNOUT AREA (18mX6m FROM POR) MUST BE PROVIDED.
 12. TERMINAL MOUNTING HEIGHT SHALL MATCH THE LONGITUDINAL BARRIER MOUNTING HEIGHT.



REFER NOTE 9 AND 10

VicRoads Drawing No. 720267

- REFERENCES AND NOTES:**
1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS SHOWN OTHERWISE.
 2. MANUFACTURERS TERMINAL TECHNICAL DRAWINGS.
 3. VICROADS SUPPLEMENT TO AUSTRROADS GUIDE TO ROAD DESIGN - PART 6.
 4. REFER TO RDN 06-04 FOR ACCEPTED SAFETY BARRIER PRODUCTS.
 5. SAFETY BARRIER TERMINOLOGY, SHORTHAND AND GENERAL REQUIREMENTS SHALL BE IN ACCORDANCE WITH SD 3500.

INVESTMENT AND DESIGN SERVICES
 MANAGER
 SAFE SYSTEM DESIGN
 60 DENMARK STREET
 KEW
 VICTORIA 3101
 PHONE (03) 9854 2666



ROAD SAFETY BARRIERS			
GATING REDIRECTIVE ENERGY ABSORBING TERMINAL			
RUNOUT AREA DETAILS			
NOT TO SCALE	APPROVED D.CASSAR (PRDE)	SD NO. 8-8-13 SD 3545	ISSUE B